



Planning Approval Consistency Assessment Form

SM-17-00000111

Metro Body of Knowledge (MBoK)

Assessment name:	SM Package 4 HSE MCL Mobile Crane Set up on Broughton Street and associated road closure
Prepared by:	Ryan O'Leary - HSEJV
Prepared for:	Sydney Metro
Assessment number:	SWM25
Status:	Final
Version:	B
Planning approval:	SSI 8256
Date required:	28/01/2022
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For information – do not alter:

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Document Owner:	Director, Environment, Sustainability & Planning
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Table of contents

1. Existing Approved Project	3
2. Description of proposed development/activity/works	6
3. Timeframe.....	9
4. Site description	9
5. Site Environmental Characteristics	10
6. Justification for the proposed works.....	10
7. Environmental Benefit.....	11
8. Control Measures.....	11
9. Climate Change Impacts	11
10. Impact Assessment – Construction	12
11. Impact Assessment – Operation	19
12. Consistency with the Approved Project	21
13. Other Environmental Approvals.....	22
Author certification	23
Appendix A – Site Location	25
Appendix B – ROLs and TCPs.....	26
Appendix C – Community Notification	27

The Planning Approval Consistency Assessment Form should be completed in accordance with [SM-17-00000103 Planning Approval Consistency Assessment Procedure](#).

1. Existing Approved Project

Planning approval reference details (Application/Document No. (including modifications)):

SSI 8256 Sydney Metro City & Southwest – Sydenham to Bankstown (S2B)
Mod 1 Revised station design for Bankstown Station

Date of determination:

Infrastructure Approval date 12 December 2018
Modification 1 Approval date 22 October 2020

Type of planning approval:

Critical State Significant Infrastructure

Description of existing approved project you are assessing for consistency:

The Marrickville, Canterbury and Lakemba (MCL) Station Upgrades is one of the stages of the Sydenham to Bankstown upgrade (herein referred to as the Southwest Metro (SWM) Project) as described in the project's delivery strategy. The MCL upgrades to Sydney Metro standards correspond to work package No. 4 which are being undertaken by Haslin / Stephen Edwards Joint Venture(HSE JV).

Upgrade works at Canterbury Station involves the following:

- Refurbish and repurpose rooms of existing concourse booking office, platform building 1 and 2;
- Remove existing stair & canopy to Platform 1. Provide a new lift & stair to Platform 1 including associated canopies;
- Regrade platform as per Sydney Metro's requirement and provide drainage, platform screen doors, platform edge screens and mechanical gap fillers to Platform 1 and 2;
- Provide a new lift to platform 2 including associated canopies;
- Construction of the Sydney Metro Services Building;
- Provide new security gates to concourse entry;
- New cabling and containment for LV services and lighting;
- Clad the southern side of station concourse booking office, and refurbish the building. Provide a new opening onto Canterbury Road for existing retail;
- Remove the existing planter beds to Broughton Street;
- Remove the canopy directly over the existing planter bed facing Broughton Street;
- Remove existing brick retaining wall from station concourse forecourt entry adjacent to Canterbury Road;
- Provide accessible entries from both Canterbury Road and Broughton Street to station concourse;
- Replace the existing vertical protection (anti-throw) screens to the station concourse bridge;
- Renew lighting to the concourse, footbridge, platform buildings, platforms and ramp to Platform 2;

- Repair the existing booking office roof and associated stormwater system. Repaint, repoint and repair existing platform buildings;
- Replace existing balustrade on Platform 2 ramp and continue new fencing to platform building 2 with new. Resurface asphalt finish to Platform 2 ramp and contain asphalt edges with steel flat bar;
- Installation of new CSR cable route;
- Installation of security and segregation fencing;
- Canterbury Road bridge parapet works (city and country side); and
- Replacement of existing bus shelters on Broughton Street.

During the initial approvals process it was assumed that construction activities would occur along the length of the rail corridor and that all construction areas would be accessed via existing corridor gates. Section 10.3.3 of the EIS identified that changes to the road network, including temporary road and lane closures, around stations would be required as a result of construction. Table 10.35 of the EIS identified potential changes to the road network for station works however, this would change during detailed design and construction planning.

It should also be noted that the SPIR identified key changes to the indicative construction methodology for the preferred project (compared to the exhibited project in the EIS) to support a design solution that reduced community impacts. The SPIR Submissions Report identified, based on the indicative construction methodology, that no full road closures would be required during the station upgrade works. The EIS/ SPIR noted that final construction methodology would be determined by the construction contractor once appointed.

This Planning Approval Consistency Assessment has been produced to assess potential impacts of temporary full road closure of Broughton Street associated with Canterbury Station upgrades to Sydney Metro standards, and to determine whether those impacts can be appropriately managed under the current Conditions of Approval, Revised Environmental Mitigation Measures, management plans, procedures and strategies.

Relevant background information (including EA, REF, Submissions Report, Director General's Report, MCoA):

- The Sydney Metro City & Southwest – Sydenham to Bankstown – State Significant Infrastructure Assessment (SSI 8256), dated 12th December 2018
- The Sydney Metro City & Southwest – Sydenham to Bankstown – Environmental Impact Statement, dated 7th September 2017;
- The Sydney Metro City & Southwest – Sydenham to Bankstown – Submissions and Preferred Infrastructure Report, June 2018;
- The Sydney Metro City & Southwest – Sydenham to Bankstown – Submissions Report, September 2018;
- The Sydney Metro City & Southwest – Sydenham to Bankstown – Instrument of Approval, dated 12th December 2018
- The Sydney Metro City & Southwest – Sydenham to Bankstown – Modification 1 – Bankstown Station, 22nd October 2020.

All proposed works identified in this assessment would be undertaken in accordance with the mitigation measures identified in the EIS, Submissions and Preferred Infrastructure Report, the Submission Report and the conditions of approval.

2. Description of proposed development/activity/works

Describe ancillary activities, duration of work, working hours, machinery, staffing levels, impacts on utilities/authorities, wastes generated or hazardous substances/dangerous goods used.

This Planning Approval Consistency Assessment (CA) relates to the temporary full road closure of Broughton Street, Canterbury, between Canterbury Road and Robert Street during possession works for the establishment of a mobile crane. The pedestrian footpath on the southern side of Broughton St, between Canterbury Road and Robert Street will be closed to ensure members of the public do not enter the work zone and walk below the crane as it lifts and installs materials into the station. The pedestrian footpath on the northern side of the street would be maintained during these works.

Broughton Street is located within the City of Canterbury Bankstown Council Local Government Area (LGA). Appendix A provides a map of the proposed road closure area and traffic controls planned to be in place.

It is noted that Broughton Street already falls under the approved project area and this CA has been produced to assess the consistency of the full road closure activity. Use of the crane is also approved by the Council which falls under the Road Occupancy Licence (ROL).

Closure of Broughton Street would only be required during possession works which would take place under the relevant Out of Hours Works Application prepared by HSE and approved by Sydney Metro.

The proposed closure will restrict parking on Broughton Street between Canterbury Road and 25 metres west of Robert Street on both sides of the carriageway. The existing conditions reveal that this area is predominantly a “Bus Zone”, and no parking is permitted. The detour swept paths also indicate that additional parking removal is required to facilitate safe heavy vehicle movements. Therefore, this closure will impact some kerb side car park spaces whilst the closure is in place.

The proposed area in Appendix A is on land owned by the City of Canterbury Bankstown Council and a relevant ROL and Traffic Control Plan (TCP) has been approved, located in Appendix B.

Site utes will be used by traffic controllers as part of the full road closure works. Plant and equipment, and the like, to be used within the closed area of Broughton Street and to access the rail corridor is not expected to differ from the approved construction methodology. For information, this would include, but not limited to the following indicative plant/machinery:

- 400T mobile crane
- 25T franna crane
- Elevated work platform
- Delivery trucks
- Generator
- 2x Lift shafts
- 2x Pre-cast stair units
- Glazing and structural steel components
- Tool trucks / site utes
- 2x Lighting towers.

Approximately 50 workers will be accessing this area. Traffic controller crew will also be managing the road closure as detailed in the Traffic Control Plan.

Isolating two aerials associated with the bus shelter and public toilet. With power to the existing toilet facility being disconnected, portaloos will be installed (setup inside the road closure area) for public use during the possession while the existing toilet is closed. Utilities will be reconnected prior to the end of each possession of works, being 21 March 2022 and 28 March 2022, respectively.

The only waste associated with the full road closure activity or lane closure will be waste water from the portaloos. The portaloos will be serviced, as required, by the contractor. Waste water will be pumped into a truck and disposed of offsite to a waste facility licensed to accept the waste water.

Fuel may be required for the crane, generators and lighting towers. Fuel will be stored in a bund and spill kit will be located within the work area.

Upon completion of works the proposed area would be reinstated to the public.

3. Timeframe

When will the proposed change take place? For how long?

A full road closure of Broughton Street will be required for weekend 38 and 39 possession works during the following periods:

- Friday 18 March 2022 to Monday 21 March 2022 and
- Friday 25 March 2022 to Monday 28 March 2022.

4. Site description

Provide a description of the site on which the proposed works are to be carried out, including, Lot and Deposited Plan details, where available. Map to be included here or as an appendix. Detail of land owner.

The proposed area is located within the road reserve on land owned by the City of Canterbury Bankstown Council. As such there are no Lot and Deposited Plan details. Refer to Section 5 below for Site Environmental Characteristics.

5. Site Environmental Characteristics

Describe the environment (i.e., vegetation, nearby waterways, land use, surrounding land use), identify likely presence of protected flora/fauna and sensitive area.

The environment at Broughton Street, Canterbury can be described as typical urban street scape. The roadway is bordered by gutters, footpath, private property and the rail corridor. Nearby vegetation consists of planted trees along the corridor side of Broughton Street. Trees 595, 596 and 597 are required to be pruned to allow for clearance of the mobile crane and its movement. Tree protection around the trunks and surrounding branches will be implemented as per the arborist report sent to DPIE.

Rainfall runoff from the area enters stormwater pits located within the kerb side gutter on either side of Broughton Street. Two stormwater pits are located within the area proposed for closure, with one on the northern side of Broughton Street, approx. 7 meters from Robert Street, and a second one on the southern side opposite Robert Street. Land surrounding the road closure area consists of mixed-use and residential properties, predominantly residential, and the rail corridor and station precinct to the southwest. Broughton Street adjoins with Canterbury Road, which is a main road.

There is no known protected flora or fauna in the vicinity. There are no known other sensitive receivers in the area.

HSE JV will restore the area to its pre-existing condition upon completion of the works.

6. Justification for the proposed works

Address the need for the proposed works, whether there are alternatives to the proposed works (and why these are not appropriate), and the consequences with not proceeding with the proposed work.

The size and weight of the lift and stairs require a significant size mobile crane to install (see total weights below). Elements cannot be installed from the rail corridor track due to the large size of the elements and therefore are required to be installed from an adjacent road.

ROs are not feasible on Canterbury Road, as the mobile crane would require minimum 2 lane closure.

7. Environmental Benefit

Identify whether there are environmental benefits associated with the proposed works. If so, provide details:

Nil.

8. Control Measures

Will a project and site specific EMP be prepared? Are appropriate control measures already identified in an existing EMP?

Works will be completed under the project Construction Traffic Management Plan (CTMP), Construction Environmental Management Plan (CEMP) and sub-plans, including the Construction Noise and Vibration Management Plan (CNVMP), Construction Heritage Management Plan (CHMP), Construction Soil and Water Management Plan (CSWMP), and Community Consultation Strategy (CCS).

9. Climate Change Impacts

Is the site likely to be adversely affected by the impacts of climate change? If yes, what adaptation/mitigation measures will be incorporated into the design?

No changes to climate change impacts.

10. Impact Assessment – Construction

Attach supporting evidence in the Appendices if required. Make reference to the relevant Appendix if used.

Aspect	Nature and extent of impacts (negative and positive) during construction (if control measures implemented) of the proposed/activity, relative to the Approved Project	Proposed Control Measures in addition to project COA and REMMs	Minimal Impact Y/N	Endorsed	
				Y/N	Comments
Flora and fauna	To allow for clearance of the mobile crane and its movements, tree pruning will be required for trees 595, 596 and 597 (all <i>Cinnamomum camphora</i>) as identified within the arborist report. The Arborist report was issued to DPIE for information purposes. These trees are not native, and as such, will not require the attendance of a qualified ecologist.	Pre-clearance checklist and permit to clear will be completed prior to pruning. Tree protection for trucks and surrounding branches will be implemented as per the arborist report.	Y	Y	
Water	No change from approved project as detailed in the EIS and SPIR.	No change from the EIS and SPIR. Erosion and Sediment Control Plans shall be updated to include controls required in this area. Comply with mitigation measures as stated within the CEMP and CEMP sub-plans.	Y	Y	
Air quality	There will be minor localised dust impacts from vehicle and material movements, the extent of which is considered to be consistent with the impacts assessed within the EIS/SPIR.	No additional measures required.	Y	Y	

Aspect	Nature and extent of impacts (negative and positive) during construction (if control measures implemented) of the proposed/activity, relative to the Approved Project	Proposed Control Measures in addition to project COA and REMMs	Minimal Impact Y/N	Endorsed	
				Y/N	Comments
Noise vibration	Works taking place in this area would not differ from the EIS and SPIR and consequently noise and vibration impacts are expected to be within the levels assessed in the EIS/SPIR for construction works. The same receivers will be impacted and there could be some additional noise from the setup of traffic management controls and from use of the crane. All work outside of standard construction hours would be assessed under an OOHW Application. Additional Mitigation Measures as per the Construction Noise and Vibration Strategy (CNVS) (i.e. community consultation and notifications).	Implementation of control measures as per the CEMP, CNVMP and OOHW. Mitigation and respite will be applied in accordance with the CNVMP and Sydney Metro CNVS.	Y	Y	
Aboriginal heritage	No change from the EIS and SPIR.	No additional measures required.	Y	Y	
Non-Aboriginal heritage	No change from the EIS and SPIR.	No additional measures required.	Y	Y	

Aspect	Nature and extent of impacts (negative and positive) during construction (if control measures implemented) of the proposed/activity, relative to the Approved Project	Proposed Control Measures in addition to project COA and REMMs	Minimal Impact Y/N	Endorsed	
				Y/N	Comments
Community and stakeholder	<p>Rerouting of traffic during road closure may cause temporary disruption to community members and stakeholders, particularly those accessing properties along Broughton Street between Canterbury Road and Robert Street.</p> <p>Pedestrian access through Broughton Street would be limited to the footpath on the northern side of the street.</p> <p>Existing public toilet adjacent to the station will be closed for the duration of the road closure. Portaloo toilets will be installed for public use during the possession while the existing toilet is closed.</p> <p>Proposed works may cause visual impacts. Refer to Visual Impact section for assessment.</p> <p>Ongoing community consultation has been taking place. See Appendix C for 'Keeping You Informed' notification. An updated notification will be included in either the monthly notification for March 2022 to be distributed at the end of February 2022 or a specific notification for the specified work periods.</p>	<p>Ongoing consultation and notification as per the Community Communications Strategy (CCS).</p> <p>Implementation of control measures as per the CEMP, CEMP sub-plans, CCS and CTMP.</p>	Y	Y	

<p>Traffic</p>	<p>The proposed closure will be deployed during 24-hour traffic control setups on Broughton Street, Robert Street and Jeffrey Street.</p> <p>The following kerb side car park spaces will be utilised whilst the closure is in place:</p> <ul style="list-style-type: none"> • 5 car parks adjacent to Robert Street will be removed to provide adequate space for two buses to navigate the Robert Street and Broughton Street intersection. • 5 car parks will be removed on the eastern side of Broughton Street (north of Robert Street). This space will act as a temporary bus stop for southbound travelling buses. • 5 car parks will be removed on the western side of Broughton Street (north of Robert Street). This space will act as a temporary bus stop for northbound travelling buses. • 8 car parks will be removed on Robert Street (east bound). This space will provide adequate width for heavy vehicles utilising the detour. • 5 car parks will be removed on Jeffrey Street (adjacent to Robert Street in the southbound direction). This space will provide adequate room to cater to movements in and out of Robert Street from Jeffrey Street. <p>Traffic control will be in place to assist local residents to access private driveways that are located within the specified work area. ROLs and TCPs are located in Appendix B.</p> <p>The existing conditions reveal that a right turn prohibition is in place for southbound motorists on Canterbury Road that wish to turn right into Jeffrey Street. Therefore, an alternative route for southbound motorists to travel to Broughton Street is via Unwin Road, Jeffrey Street and Robert Street.</p>	<p>Implementation of control measures as per the CEMP and CTMP. The CTMP will be updated to include the lane area once approved.</p> <p>A valid ROL issued by the City of Canterbury Bankstown Council must be in place – any requirements of this permit must be implemented.</p> <p>TCPs must be implemented, including appropriate signage and traffic controllers as required.</p> <p>Consultation with any agencies identified within REMM TC3 will occur.</p>	<p>Y</p>	<p>Y</p>	
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Aspect	Nature and extent of impacts (negative and positive) during construction (if control measures implemented) of the proposed/activity, relative to the Approved Project	Proposed Control Measures in addition to project COA and REMMs	Minimal Impact Y/N	Endorsed	
				Y/N	Comments
	<p>Each of the proposed detour routes for the Broughton Street closure has had a swept path analysis conducted (as per the CoA and CTMP Section 4.2.7) to ensure that potential conflicts with both parking and general traffic movements are mitigated.</p> <p>The swept path analysis reveals that all access turn movements are suitable for buses (12.5m single unit truck). Temporary parking removal will be required to facilitate longer vehicles. Refer to Appendix B for the swept path analysis.</p> <p>The CTMP takes into account surrounding roads which are capable of handling additional traffic. For the car spaces to be temporarily utilised, as mentioned above, there is no alternative parking spaces available in the immediate vicinity. Note that pedestrian access will be maintained.</p> <p>Buses and site heavy vehicles may have to traverse over mountable median islands at the intersection of Unwin Road and Jeffrey Street. However, traffic control will be in place at this intersection to assist with these movements.</p> <p>The number of movements expected is low.</p>				
Waste	No change from the EIS and SPIR.	No additional measures required.	Y	Y	
Social	As above for Community and Stakeholder.	No additional measures required.	Y	Y	

Aspect	Nature and extent of impacts (negative and positive) during construction (if control measures implemented) of the proposed/activity, relative to the Approved Project	Proposed Control Measures in addition to project COA and REMMs	Minimal Impact Y/N	Endorsed	
				Y/N	Comments
Economic	No change from the EIS and SPIR. Access to the café at 193 Canterbury Road will be maintained. Consultation via notice has been undertaken by the HSEJV Communications Team with the café and no response has been received to date.	No additional measures required.	Y	Y	
Visual	The establishment and movements of the mobile crane, vehicles, equipment, plant, signage and barricading will be highly visible. The visual aspects of these activities is to be expected as part of a major construction project and an operating rail corridor. These would be temporary and general works for the project have been considered in the Project Landscape and Visual Assessment Report. Some light spill may occur from the works, either from lighting towers or heavy vehicles. Light spill will be minimised by pointing lights away from residential properties and the roadway, towards the works.	Visual impacts are to be managed in accordance with the Visual Amenity Management Plan. Lighting towers will be aimed away from residential receivers to minimise light spill. Headlights from construction vehicles will be switched off when not in use.	Y	Y	
Urban design	No change from the EIS and SPIR	No additional measures required.	Y	Y	
Geotechnical	No change from the EIS and SPIR	No additional measures required.	Y	Y	
Land use	No change from the EIS and SPIR.	No additional measures required.	Y	Y	
Climate Change	No change from the EIS and SPIR	No additional measures required.	Y	Y	
Risk	No change from the EIS and SPIR	No additional measures required.	Y	Y	

Aspect	Nature and extent of impacts (negative and positive) during construction (if control measures implemented) of the proposed/activity, relative to the Approved Project	Proposed Control Measures in addition to project COA and REMMs	Minimal Impact Y/N	Endorsed	
				Y/N	Comments
Other	No change from the EIS and SPIR	No additional measures required.	Y	Y	
Management and mitigation measures	No change from the EIS and SPIR	No additional measures required.	Y	Y	

11. Impact Assessment – Operation

Attach supporting evidence in the Appendix if required. Make reference to the relevant Appendix if used.

Aspect	Nature and extent of impacts (negative and positive) during operation (if control measures implemented) of the proposed activity/works, relative to the Approved Project	Proposed Control Measures in addition to project COA and REMMs	Minimal Impact Y/N	Endorsed	
				Y/N	Comments
Flora and fauna	No change from the EIS and SPIR	N/A	Y	Y	
Water	No change from the EIS and SPIR	N/A	Y	Y	
Air quality	No change from the EIS and SPIR	N/A	Y	Y	
Noise vibration	No change from the EIS and SPIR	N/A	Y	Y	
Aboriginal heritage	No change from the EIS and SPIR	N/A	Y	Y	
Non-Aboriginal heritage	No change from the EIS and SPIR	N/A	Y	Y	
Community and stakeholder	No change from the EIS and SPIR	N/A	Y	Y	
Traffic	No change from the EIS and SPIR	N/A	Y	Y	
Waste	No change from the EIS and SPIR	N/A	Y	Y	
Social	No change from the EIS and SPIR	N/A	Y	Y	
Economic	No change from the EIS and SPIR	N/A	Y	Y	
Visual	No change from the EIS and SPIR	N/A	Y	Y	

Aspect	Nature and extent of impacts (negative and positive) during operation (if control measures implemented) of the proposed activity/works, relative to the Approved Project	Proposed Control Measures in addition to project COA and REMMs	Minimal Impact Y/N	Endorsed	
				Y/N	Comments
Urban design	No change from the EIS and SPIR	N/A	Y	Y	
Geotechnical	No change from the EIS and SPIR	N/A	Y	Y	
Land use	No change from the EIS and SPIR	N/A	Y	Y	
Climate Change	No change from the EIS and SPIR	N/A	Y	Y	
Risk	No change from the EIS and SPIR	N/A	Y	Y	
Other	No change from the EIS and SPIR	N/A	Y	Y	
Management and mitigation measures	No change from the EIS and SPIR	N/A	Y	Y	

12. Consistency with the Approved Project

Based on a review and understanding of the existing Approved Project and the proposed modifications, is there is a transformation of the Project?	No. The proposed works would not transform the project, they will facilitate it. The project would continue to provide a new metro rail line between Sydenham and Bankstown.
Is the project as modified consistent with the objectives and functions of the Approved Project as a whole?	Yes. The proposed works would be consistent with the objectives and functions of the approved project.
Is the project as modified consistent with the objectives and functions of elements of the Approved Project?	Yes. The changes identified in this assessment are temporary and are consistent with the objectives and functions of the Approved Project.
Are there any new environmental impacts as a result of the proposed works/modifications?	No new environmental impacts are introduced as part of the proposed use of Broughton Street. All impacts are adequately addressed through the application of the mitigation measures in the above tables, the EIS and construction environmental management plan for the project.
Is the project as modified consistent with the conditions of approval?	Yes. The proposed works would be consistent with the conditions of approval.
Are the impacts of the proposed activity/works known and understood?	Yes. The impacts of the proposed works are understood.
Are the impacts of the proposed activity/works able to be managed so as not to have an adverse impact?	Yes. The impacts of the proposed works can be managed so as to avoid an adverse impact.

13. Other Environmental Approvals

Identify all other approvals required for the project:

Consultation with the City of Canterbury Bankstown City Council has been undertaken, and Traffic Control Plans and Road Occupancy Licenses approved. Council has noted support for the temporary use of Broughton Street see Appendix B.

Author certification

To be completed by person preparing checklist.

I certify that to the best of my knowledge this Consistency Checklist:

- Examines and takes into account the fullest extent possible all matters affecting or likely to affect the environment as a result of activities associated with the Proposed Revision; and
- Examines the consistency of the Proposed Revision with the Approved Project; is accurate in all material respects and does not omit any material information.

Name:	Ryan O'Leary	Signature:	<i>Ryan O'Leary</i>
Title:	Environment Manager		
Company:	HSE JV	Date:	18/01/2022

This section is for Sydney Metro only.

Application supported and submitted by

Name:	Yvette Buchli	Date:	21/01/2022
Title:	Associate Director Planning Approvals	Comments:	
Signature:	<i>Yvette Buchli</i>		

Based on the above assessment, are the impacts and scope of the proposed activity/modification consistent with the existing Approved Project?

- Yes The proposed activity/works are consistent and no further assessment is required.
- No The proposed works/activity is not consistent with the Approved Project. A modification or a new activity approval/ consent is required. Advise Project Manager of appropriate alternative planning approvals pathway to be undertaken.

Endorsed by			
Name:	Fil Cerone	Date:	27 January 2022
Title:	Director, City & Southwest, Sustainability Environment and Planning	Comments:	
Signature:			

Appendix A – Site Location



Figure 1 – Indicative area of Broughton Street closure location



Appendix B – ROLs and TCPs

ROAD OCCUPANCY LICENCE

LICENCE NO : 1716574

TRANSPORT MANAGEMENT CENTRE (TMC)

Phone: 0283961513 Monday To Friday 8.30 AM - 4.30 PM



To activate and deactivate your approved work shift(s) on your Road Occupancy Licence, please visit: myrol.transport.nsw.gov.au. This licence is for the occupation of the road space only. If you are unable to access myrol.transport.nsw.gov.au, please call TMC on 1800 679 782. For further assistance, please refer to the proponent's user manual here: myrol.transport.nsw.gov.au/help.pdf

DEVELOPMENT - REPAIR

Project: Sydney Metro West Project
This Activity : South West Metro Package 4 - Canterbury Station Upgrade Works - weekend closure of Broughton Street (Canterbury City Council Road)

LOCATION

Subject Road: Broughton Street
From: Canterbury Road, CANTERBURY
To: Robert Street, CANTERBURY
Council: CANTERBURY-BANKSTOWN

LICENSEE

Organisation: Civlink Consulting Pty Ltd
Ref No:
Name: Louis Peau
Phone: 0401511877

ONSITE CONTACT

Name: Vitor Reis
Phone: 0447124014

TRAFFIC MANAGEMENT

Flow Management: Detour (other roads)
Closure Type: All lanes both directions
Closure Lane(s): Shoulder; Median Shoulder
Direction(s): Northbound and Southbound

LICENCE DURATION

From: 07-Jan-2022
To: 10-Jan-2022

LICENCE CONDITIONS

- 1 AN OPLINC SYSTEM OUTAGE HAS BEEN PLANNED FOR TUESDAY 2 NOVEMBER 2021 TO TUESDAY 9 NOVEMBER 2021. DURING THE OUTAGE NEW PROPONENT REGISTRATIONS AND BOTH NEW AND EXTENSION ROAD OCCUPANCY LICENCE (ROL) APPLICATION SUBMISSIONS IN THE OPLINC SYSTEM WILL BE UNAVAILABLE. THE OUTAGE WILL BE IN PREPARATION FOR THE IMPLEMENTATION OF A NEW ROAD OCCUPANCY APPLICATION AND SHIFT MANAGEMENT PORTAL FROM WEDNESDAY 10 NOVEMBER 2021. THE NEW PORTAL IS ALIGNED WITH THE TFNSW GOAL OF PROVIDING OUR CUSTOMERS IMPROVED NETWORK EFFICIENCY. ALL NEW ROL APPLICATIONS AND ANY EXISTING ROLS THAT REQUIRE EXTENSIONS DURING THE OUTAGE MUST BE SUBMITTED VIA OPLINC NO LATER THAN TUESDAY 19 OCTOBER 2021. FROM WEDNESDAY 10 NOVEMBER 2021, ALL NEW PROPONENT REGISTRATIONS AND ALL TYPES OF ROL APPLICATIONS WILL BE PROCESSED WITHIN 10 BUSINESS DAYS FROM THE DATE OF SUBMISSION. DURING THE OUTAGE, SHIFT ACTIVATION/DEACTIVATIONS WILL CONTINUE TO BE AVAILABLE AND THE CURRENT PROCESS WILL REMAIN THE SAME.
- 2 YOU MUST USE SHIFT ACTIVATION WEB ADDRESS <https://myrol.transport.nsw.gov.au> TO ACTIVATE AND DEACTIVATE YOUR APPROVED ROAD OCCUPANCY LICENCE(S). (TO CHANGE TRAFFIC CONTROL SIGNALS TO FLASHING YELLOW OR TO ACTIVATE PERMANENT VARIABLE MESSAGE SIGNS DIAL 1800 679 782)
- 3 THIS LICENCE IS NOT AN APPROVAL OF THE PROPONENT'S TRAFFIC GUIDANCE SCHEMES (TGS). PLEASE NOTE WORKCOVER REQUIRES THAT TRAFFIC GUIDANCE SCHEMES (TGS) COMPLY WITH AS1742.3
- 4 ALL MATTERS RELATING TO NOISE GENERATION OR OTHER ENVIRONMENTAL FACTORS ON SITE ARE UNDER THE JURISDICTION OF THE LOCAL COUNCIL AND/OR THE ENVIRONMENTAL PROTECTION AUTHORITY.
- 5 SHOULD THE PROPOSED WORKS INVOLVE UNDERBORING OR EXCAVATION OF STATE ROAD ASSETS OR THE REMOVAL OF KERB AND GUTTER, DETAILS OF WORKS MUST BE APPROVED BY TFNSW. FOR GREATER SYDNEY REGION CONTACT: greaterSydneyroads@transport.nsw.gov.au. FOR REGIONAL & OUTER METROPOLITAN. CONTACT: road.access@transport.nsw.gov.au.
- 6 NOTIFICATION TO AFFECTED BUSINESSES, RESIDENTS AND OTHER STAKEHOLDERS MUST BE UNDERTAKEN AT LEAST 5 BUSINESS DAYS PRIOR TO WORKS COMMENCING
- 7 IF ROAD WORKS ARE IN THE VICINITY OF TRAFFIC CONTROL SIGNALS, PEDESTRIAN ACCESS TO PUSH BUTTONS MUST BE AVAILABLE AT ALL TIMES OR ALTERNATE ARRANGEMENTS MUST BE MADE TO ASSIST THE SAFE CROSSING OF PEDESTRIANS.

APPROVED DATES & TIMES

From	From Shift			-	To Shift			
	D	M	Time		D	M	Time	
Fri	07	Jan	20:00	-	Sat	08	Jan	00:00
Sat	08	Jan	00:00	-	Sun	09	Jan	00:00
Sun	09	Jan	00:00	-	Mon	10	Jan	00:00
Mon	10	Jan	00:00	-	Mon	10	Jan	05:00

ROAD OCCUPANCY LICENCE

LICENCE NO : 1716574

TRANSPORT MANAGEMENT CENTRE (TMC)

Phone: 0283961513 Monday To Friday 8.30 AM - 4.30 PM



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DEVELOPMENT - REPAIR

Project: Sydney Metro West Project
This Activity : South West Metro Package 4 - Canterbury Station Upgrade Works - weekend closure of Broughton Street (Canterbury City Council Road)

LOCATION

Subject Road: Broughton Street
From: Canterbury Road, CANTERBURY
To: Robert Street, CANTERBURY
Council: CANTERBURY-BANKSTOWN

LICENSEE

Organisation: Civlink Consulting Pty Ltd
Ref No:
Name: Louis Peau
Phone: 0401511877

ONSITE CONTACT

Name: Vitor Reis
Phone: 0447124014

TRAFFIC MANAGEMENT

Flow Management: Detour (other roads)
Closure Type: All lanes both directions
Closure Lane(s): Shoulder; Median Shoulder
Direction(s): Northbound and Southbound

LICENCE DURATION

From: 07-Jan-2022
To: 10-Jan-2022

LICENCE CONDITIONS

- 8 PEDESTRIAN ACCESS AT PRAM RAMPS MUST BE MAINTAINED AT ALL TIMES AND IF ALTERED MUST COMPLY WITH TNSW'S TECHNOLOGY STANDARDS AND OTHER RELEVANT AUSTRALIAN STANDARDS OR RELEVANT ROAD AUTHORITY REQUIREMENTS.
- 9 PROPONENT MUST IMPLEMENT THIS PLAN AS PER APPROVED TMP
- 10 LETTER BOX DROP NOTIFICATION MUST BE UNDERTAKEN WITH LOCAL RESIDENTS/BUSINESSES AFFECTED BY THE CLOSURE AT LEAST 1 WEEK PRIOR TO WORKS COMMENCING. SEE COUNCIL REQUIREMENTS
- 11 PEDESTRIAN ACCESS MUST BE MAINTAINED AROUND THE WORK AREA IN A SAFE MANNER, COMPLYING WITH COUNCIL REQUIREMENTS.
- 12 SIGNAL OPERATIONS MUST BE DISCUSSED WITH TMC PRIOR TO UNDERTAKING CLOSURE.
- 13 CLOSE CONTACT MUST BE MAINTAINED WITH TRANSPORT MANAGEMENT CENTRE ON 1300 725 886 DURING THE WORKS.
- 14 VARIABLE MESSAGE SIGNS MUST BE INSTALLED AT LEAST 5 WORKING DAYS PRIOR TO OPERATIONS COMMENCING. LOCATIONS AND MESSAGES MUST BE APPROVED BY THE TNSW'S NETWORK ACCESS TEAMS.
- 15 FOR ALL WORKS IMPACTING BUS ROUTES OR STOPS, CJPO ARE TO BE CONSULTED. PLEASE CONTACT
RABIH.BEKDACHE@TRANSPORT.NSW.GOV.AU OR
FRANKIE.PASSARELLI@TRANSPORT.NSW.GOV.AU. OR
• ELIZABETH.HARRISON@TRANSPORT.NSW.GOV.AU &
CRAIG.DUNN@TRANSPORT.NSW.GOV.AU AND OBTAIN THEIR CONCURRENCE.
•
- 16 THIS LICENCE OPERATES IN CONJUNCTION WITH A SPEED LIMIT REDUCTION TO 40 KM/H. REFER TO SZA TT/TMC 1716574/001

SPEED ZONE AUTHORISATION

LIC/SZA NO : 1716574/001

TRANSPORT MANAGEMENT CENTRE (TMC)

Phone: 0283961513 Monday To Friday 8.30 AM - 4.30 PM



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DEVELOPMENT - REPAIR

Project: Sydney Metro West Project
This Activity : South West Metro Package 4 - Canterbury Station
Upgrade Works - weekend closure of Broughton Street (Canterbury City Council Road)

SPEED LIMIT REDUCTION

Existing: 60 Km/h
Reduced To : 40 Km/h

This Speed limit is ONLY to be applied during the approved time periods listed. Signs are to be covered or removed outside the approved time periods. The Organisation is to maintain accurate records of when the speed limit signs were installed and removed, including the location of the signs, the date and times. The existing posted speed limit* is to be reinstated at the completion of each of the approved time periods as detailed herein.

LOCATION

Subject Road: Broughton Street
From: Canterbury Road, CANTERBURY
To: Robert Street, CANTERBURY
Council:

TRAFFIC MANAGEMENT

Distance(m): 220
Direction: Northbound and Southbound

LICENSEE

Organisation: Civlink Consulting Pty Ltd
Ref No:
Name: Louis Peau
Phone: 0401511877

ONSITE CONTACT

Name: Vitor Reis
Phone: 0447124014

APPROVED DATES & TIMES

From	From Shift			-	To	To Shift		
	D	M	Time			D	M	Time
Fri	07	Jan	20:00	-	Sat	08	Jan	00:00
Sat	08	Jan	00:00	-	Sun	09	Jan	00:00
Sun	09	Jan	00:00	-	Mon	10	Jan	00:00
Mon	10	Jan	00:00	-	Mon	10	Jan	05:00

ROAD OCCUPANCY LICENCE

LICENCE NO : 1718912

TRANSPORT MANAGEMENT CENTRE (TMC)

Phone: 0283961513 Monday To Friday 8.30 AM - 4.30 PM



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DEVELOPMENT - NEW CONSTRUCTION

Project: Sydney Metro West Project
This Activity : South West Metro Package 4 - Canterbury Station Upgrade Works - weekend closure of Broughton Street (Canterbury City Council Road)

LOCATION

Subject Road: Broughton Street
From: Canterbury Road, CANTERBURY
To: Robert Street, CANTERBURY
Council: CANTERBURY-BANKSTOWN

LICENSEE

Organisation: Civlink Consulting Pty Ltd
Ref No:
Name: Louis Peau
Phone: 0401511877

ONSITE CONTACT

Name: Vitor Reis
Phone: 0447124014

TRAFFIC MANAGEMENT

Flow Management: Detour (other roads)
Closure Type: All lanes both directions
Closure Lane(s): Shoulder; Median Shoulder
Direction(s): Northbound and Southbound

LICENCE DURATION

From: 31-Dec-2021
To: 03-Jan-2022

LICENCE CONDITIONS

- 1 AN OPLINC SYSTEM OUTAGE HAS BEEN PLANNED FOR TUESDAY 2 NOVEMBER 2021 TO TUESDAY 9 NOVEMBER 2021. DURING THE OUTAGE NEW PROPONENT REGISTRATIONS AND BOTH NEW AND EXTENSION ROAD OCCUPANCY LICENCE (ROL) APPLICATION SUBMISSIONS IN THE OPLINC SYSTEM WILL BE UNAVAILABLE. THE OUTAGE WILL BE IN PREPARATION FOR THE IMPLEMENTATION OF A NEW ROAD OCCUPANCY APPLICATION AND SHIFT MANAGEMENT PORTAL FROM WEDNESDAY 10 NOVEMBER 2021. THE NEW PORTAL IS ALIGNED WITH THE TFNSW GOAL OF PROVIDING OUR CUSTOMERS IMPROVED NETWORK EFFICIENCY. ALL NEW ROL APPLICATIONS AND ANY EXISTING ROLS THAT REQUIRE EXTENSIONS DURING THE OUTAGE MUST BE SUBMITTED VIA OPLINC NO LATER THAN TUESDAY 19 OCTOBER 2021. FROM WEDNESDAY 10 NOVEMBER 2021, ALL NEW PROPONENT REGISTRATIONS AND ALL TYPES OF ROL APPLICATIONS WILL BE PROCESSED WITHIN 10 BUSINESS DAYS FROM THE DATE OF SUBMISSION. DURING THE OUTAGE, SHIFT ACTIVATION/DEACTIVATIONS WILL CONTINUE TO BE AVAILABLE AND THE CURRENT PROCESS WILL REMAIN THE SAME.
- 2 YOU MUST USE SHIFT ACTIVATION WEB ADDRESS <https://myrol.transport.nsw.gov.au> TO ACTIVATE AND DEACTIVATE YOUR APPROVED ROAD OCCUPANCY LICENCE(S). (TO CHANGE TRAFFIC CONTROL SIGNALS TO FLASHING YELLOW OR TO ACTIVATE PERMANENT VARIABLE MESSAGE SIGNS DIAL 1800 679 782)
- 3 THIS LICENCE IS NOT AN APPROVAL OF THE PROPONENT'S TRAFFIC GUIDANCE SCHEMES (TGS). PLEASE NOTE WORKCOVER REQUIRES THAT TRAFFIC GUIDANCE SCHEMES (TGS) COMPLY WITH AS1742.3
- 4 ALL MATTERS RELATING TO NOISE GENERATION OR OTHER ENVIRONMENTAL FACTORS ON SITE ARE UNDER THE JURISDICTION OF THE LOCAL COUNCIL AND/OR THE ENVIRONMENTAL PROTECTION AUTHORITY.
- 5 SHOULD THE PROPOSED WORKS INVOLVE UNDERBORING OR EXCAVATION OF STATE ROAD ASSETS OR THE REMOVAL OF KERB AND GUTTER, DETAILS OF WORKS MUST BE APPROVED BY TFNSW. FOR GREATER SYDNEY REGION CONTACT: greatersydneyroads@transport.nsw.gov.au. FOR REGIONAL & OUTER METROPOLITAN. CONTACT: road.access@transport.nsw.gov.au.
- 6 NOTIFICATION TO AFFECTED BUSINESSES, RESIDENTS AND OTHER STAKEHOLDERS MUST BE UNDERTAKEN AT LEAST 5 BUSINESS DAYS PRIOR TO WORKS COMMENCING
- 7 IF ROAD WORKS ARE IN THE VICINITY OF TRAFFIC CONTROL SIGNALS, PEDESTRIAN ACCESS TO PUSH BUTTONS MUST BE AVAILABLE AT ALL TIMES OR ALTERNATE ARRANGEMENTS MUST BE MADE TO ASSIST THE SAFE CROSSING OF PEDESTRIANS.

APPROVED DATES & TIMES

From	From Shift			-	To Shift			
	D	M	Time		D	M	Time	
Fri	31	Dec	20:00	-	Sat	01	Jan	00:00
Sat	01	Jan	00:00	-	Sun	02	Jan	00:00
Sun	02	Jan	00:00	-	Mon	03	Jan	00:00
Mon	03	Jan	00:00	-	Mon	03	Jan	05:00

ROAD OCCUPANCY LICENCE

LICENCE NO : 1718912

TRANSPORT MANAGEMENT CENTRE (TMC)

Phone: 0283961513 Monday To Friday 8.30 AM - 4.30 PM



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DEVELOPMENT - NEW CONSTRUCTION

Project: Sydney Metro West Project
This Activity : South West Metro Package 4 - Canterbury Station Upgrade Works - weekend closure of Broughton Street (Canterbury City Council Road)

LOCATION

Subject Road: Broughton Street
From: Canterbury Road, CANTERBURY
To: Robert Street, CANTERBURY
Council: CANTERBURY-BANKSTOWN

LICENSEE

Organisation: Civlink Consulting Pty Ltd
Ref No:
Name: Louis Peau
Phone: 0401511877

ONSITE CONTACT

Name: Vitor Reis
Phone: 0447124014

TRAFFIC MANAGEMENT

Flow Management: Detour (other roads)
Closure Type: All lanes both directions
Closure Lane(s): Shoulder; Median Shoulder
Direction(s): Northbound and Southbound

LICENCE DURATION

From: 31-Dec-2021
To: 03-Jan-2022

LICENCE CONDITIONS

- 8 PEDESTRIAN ACCESS AT PRAM RAMPS MUST BE MAINTAINED AT ALL TIMES AND IF ALTERED MUST COMPLY WITH TNSW'S TECHNOLOGY STANDARDS AND OTHER RELEVANT AUSTRALIAN STANDARDS OR RELEVANT ROAD AUTHORITY REQUIREMENTS.
- 9 PROPONENT MUST IMPLEMENT APPROVED TMP
- 10 EXISTING BUS STOPS AFFECTED BY ANY ROAD WORKS MUST BE PROVIDED WITH AN ALTERNATIVE BUS STOP OR MAINTAINED IN CONSULTATION WITH ALL BUS COMPANIES USING THESE STOPS.
- 11 THE LICENCEE OF THIS ROL MUST CONTACT THE FOLLOWING PROPONENT(S) PRIOR TO WORKS COMMENCING TO AVOID CONFLICTS WITH THEIR WORKS.
TRAFFIC LOGISTICS
PROponent - NAME MARK HAYWARD
PROponent - PHONE 0428289573
ABERGELDIE - MARIA CAMPELO - 0452 582 603
- 12 DETOUR ROUTE ON LOCAL ROADS IS SUBJECT TO APPROVAL BEING OBTAINED FROM RELEVANT LOCAL COUNCIL.
- 13 DETOUR ROUTE MUST BE CLEARLY SIGNPOSTED.
- 14 THESE WORKS ARE ONLY TO PROCEED AT THE DISCRETION OF THE TMC OPERATIONAL COORDINATION DUTY MANAGER AT THE COMMENCEMENT OF EACH SHIFT.
- 15 PEDESTRIAN ACCESS MUST BE MAINTAINED AROUND THE WORK AREA IN A SAFE MANNER, COMPLYING WITH COUNCIL REQUIREMENTS.
- 16 SIGNAL OPERATIONS MUST BE DISCUSSED WITH TMC PRIOR TO UNDERTAKING CLOSURE.
- 17 CONTACT MUST BE MADE WITH SYDNEY BUSES ON 9245 1300 PRIOR TO LANE CLOSURES BEING IMPLEMENTED.
- 18 CLOSE CONTACT MUST BE MAINTAINED WITH TRANSPORT MANAGEMENT CENTRE ON 1300 725 886 DURING THE WORKS.
- 19 THIS LICENCE OPERATES IN CONJUNCTION WITH A SPEED LIMIT REDUCTION TO 40 KM/H. REFER TO SZA TT/TMC 1718912/001

SPEED ZONE AUTHORISATION

LIC/SZA NO : 1718912/001

TRANSPORT MANAGEMENT CENTRE (TMC)

Phone: 0283961513 Monday To Friday 8.30 AM - 4.30 PM



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DEVELOPMENT - NEW CONSTRUCTION

Project: Sydney Metro West Project
This Activity : South West Metro Package 4 - Canterbury Station Upgrade Works - weekend closure of Broughton Street (Canterbury City Council Road)

SPEED LIMIT REDUCTION

Existing: 60 Km/h
Reduced To : 40 Km/h

This Speed limit is ONLY to be applied during the approved time periods listed. Signs are to be covered or removed outside the approved time periods. The Organisation is to maintain accurate records of when the speed limit signs were installed and removed, including the location of the signs, the date and times. The existing posted speed limit* is to be reinstated at the completion of each of the approved time periods as detailed herein.

LOCATION

Subject Road: Broughton Street
From: Canterbury Road, CANTERBURY
To: Robert Street, CANTERBURY
Council:

TRAFFIC MANAGEMENT

Distance(m): 220
Direction: Northbound and Southbound

LICENSEE

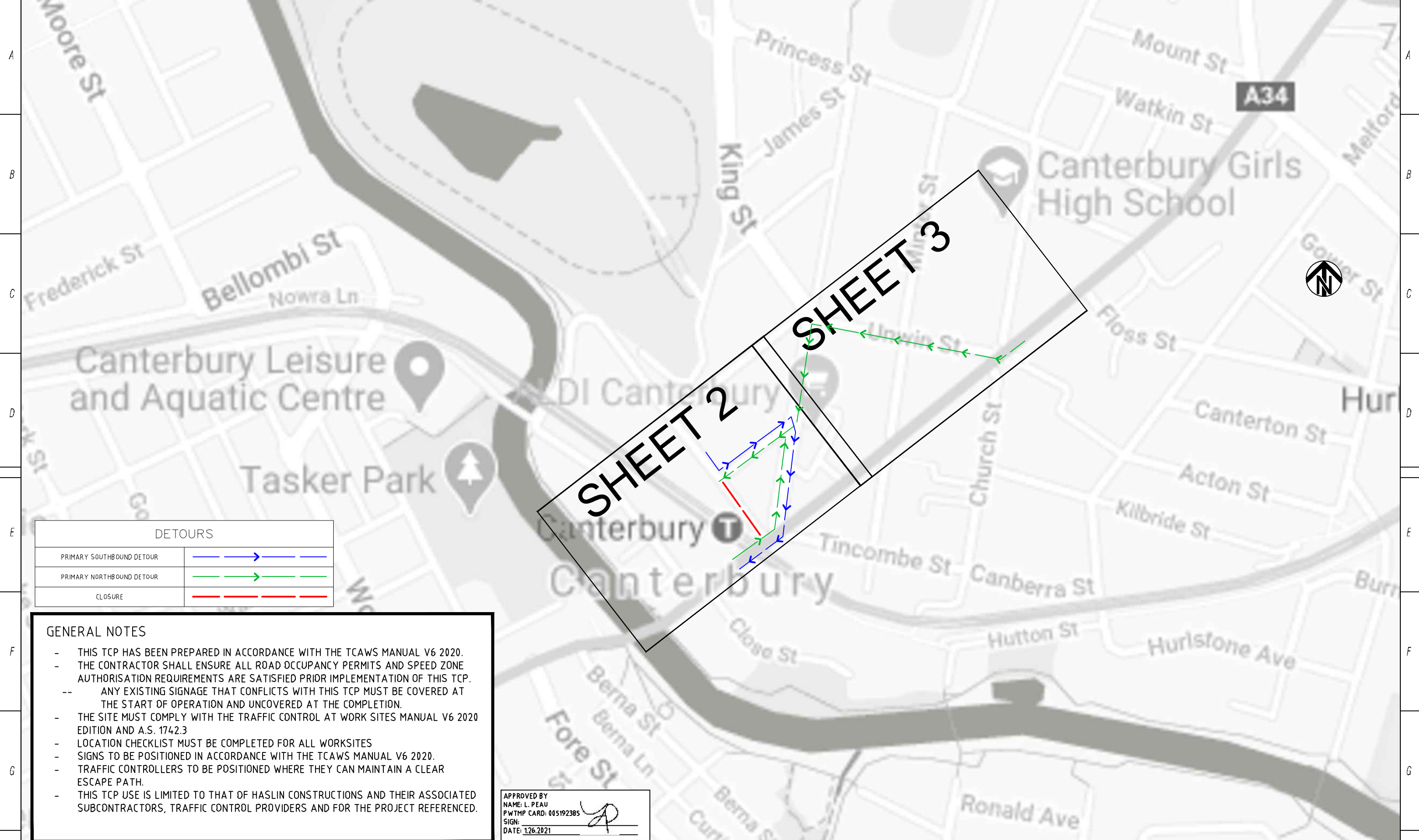
Organisation: Civlink Consulting Pty Ltd
Ref No:
Name: Louis Peau
Phone: 0401511877

ONSITE CONTACT

Name: Vitor Reis
Phone: 0447124014

APPROVED DATES & TIMES

From Shift				To Shift				
From	D	M	Time	-	To	D	M	Time
Fri	31	Dec	20:00	-	Sat	01	Jan	00:00
Sat	01	Jan	00:00	-	Sun	02	Jan	00:00
Sun	02	Jan	00:00	-	Mon	03	Jan	00:00
Mon	03	Jan	00:00	-	Mon	03	Jan	05:00



SHEET 2

SHEET 3

A
B
C
D
E
F
G
H

DETOURS	
PRIMARY SOUTHBOUND DETOUR	
PRIMARY NORTHBOUND DETOUR	
CLOSURE	

GENERAL NOTES

- THIS TCP HAS BEEN PREPARED IN ACCORDANCE WITH THE TCAWS MANUAL V6 2020.
- THE CONTRACTOR SHALL ENSURE ALL ROAD OCCUPANCY PERMITS AND SPEED ZONE AUTHORIZATION REQUIREMENTS ARE SATISFIED PRIOR IMPLEMENTATION OF THIS TCP.
- ANY EXISTING SIGNAGE THAT CONFLICTS WITH THIS TCP MUST BE COVERED AT THE START OF OPERATION AND UNCOVERED AT THE COMPLETION.
- THE SITE MUST COMPLY WITH THE TRAFFIC CONTROL AT WORK SITES MANUAL V6 2020 EDITION AND A.S. 1742.3
- LOCATION CHECKLIST MUST BE COMPLETED FOR ALL WORKSITES
- SIGNS TO BE POSITIONED IN ACCORDANCE WITH THE TCAWS MANUAL V6 2020.
- TRAFFIC CONTROLLERS TO BE POSITIONED WHERE THEY CAN MAINTAIN A CLEAR ESCAPE PATH.
- THIS TCP USE IS LIMITED TO THAT OF HASLIN CONSTRUCTIONS AND THEIR ASSOCIATED SUBCONTRACTORS, TRAFFIC CONTROL PROVIDERS AND FOR THE PROJECT REFERENCED.

APPROVED BY
NAME: L. PEAU
PWTMP CARD: 005192385
SIGN:
DATE: 1.26.2021

REV	BY	DATE	DESCRIPTION	APPD.
P4	LP	16.06.21	UPDATE SIGNAGE	LP
P3	LP	14.04.21	UPDATED PLAN TO INCLUDE UINWIN ST SIGNAGE	LP
P2	LP	8.03.21	UPDATED VMS	LP
P1	LP	30.01.21	ORIGINAL ISSUE	LP

DRAWN BY: AG
DRW CHECK: AG
APPROVED: AG
IND REVIEW: N/A

DESIGNER

CLIENT

HASLIN CONSTRUCTIONS - CANTERBURY STATION UPGRADE

BROUGHTON STREET FULL CLOSURE AND DETOUR
FROM CANTERBURY RD TO ROBERT ST
LOCALITY PLAN

DRAWING No: HAS-CAN-30005-P4

SHEET 1 OF 3

REVISION P1

SITE INSTALLATION NOTES

- SIGN INSTALLATION MUST NOT OBSTRUCT EXISTING FOOTPATHS AND SIGHT DISTANCES FOR VEHICLES TURNING OUT OF RESIDENTIAL DRIVEWAYS.
- SIGNS SHOULD BE INSTALLED AT APPROXIMATELY 90 DEGREES TO APPROACHING TRAFFIC.

LEGEND

WORK AREA	
PEDESTRIAN DETOUR ROUTE	
CLOSED PEDESTRIAN PATH	
PEDESTRIAN BARRICADE / PHYSICAL BARRIER	
TRAFFIC CONTROLLER	
TRAFFIC CONE / BOLLARD	
TEMPORARY SIGN POSITION	
SITE ACCESS / EGRESS	

C

D

E

F

G

APPROVED BY
NAME: L. PEAU
PW TMP CARD: 005192385
SIGN:
DATE: 126.2021

P4	LP	16.06.21	UPDATE SIGNAGE	LP
P3	LP	14.04.21	UPDATED PLAN TO INCLUDE WINWIN ST SIGNAGE	LP
P2	LP	8.03.21	UPDATED VMS	LP
P1	LP	30.01.21	ORIGINAL ISSUE	LP
REV	BY	DATE	DESCRIPTION	APPD.
COORDINATE SYSTEM:		HEIGHT DATUM:	SCALE:	

DRAWN BY: AG
DRW CHECK: AG
APPROVED: AG
IND REVIEW: N/A

DESIGNER
CIVILINK CONSULTING

CLIENT
HASLIN

HASLIN CONSTRUCTIONS - CANTERBURY STATION UPGRADE

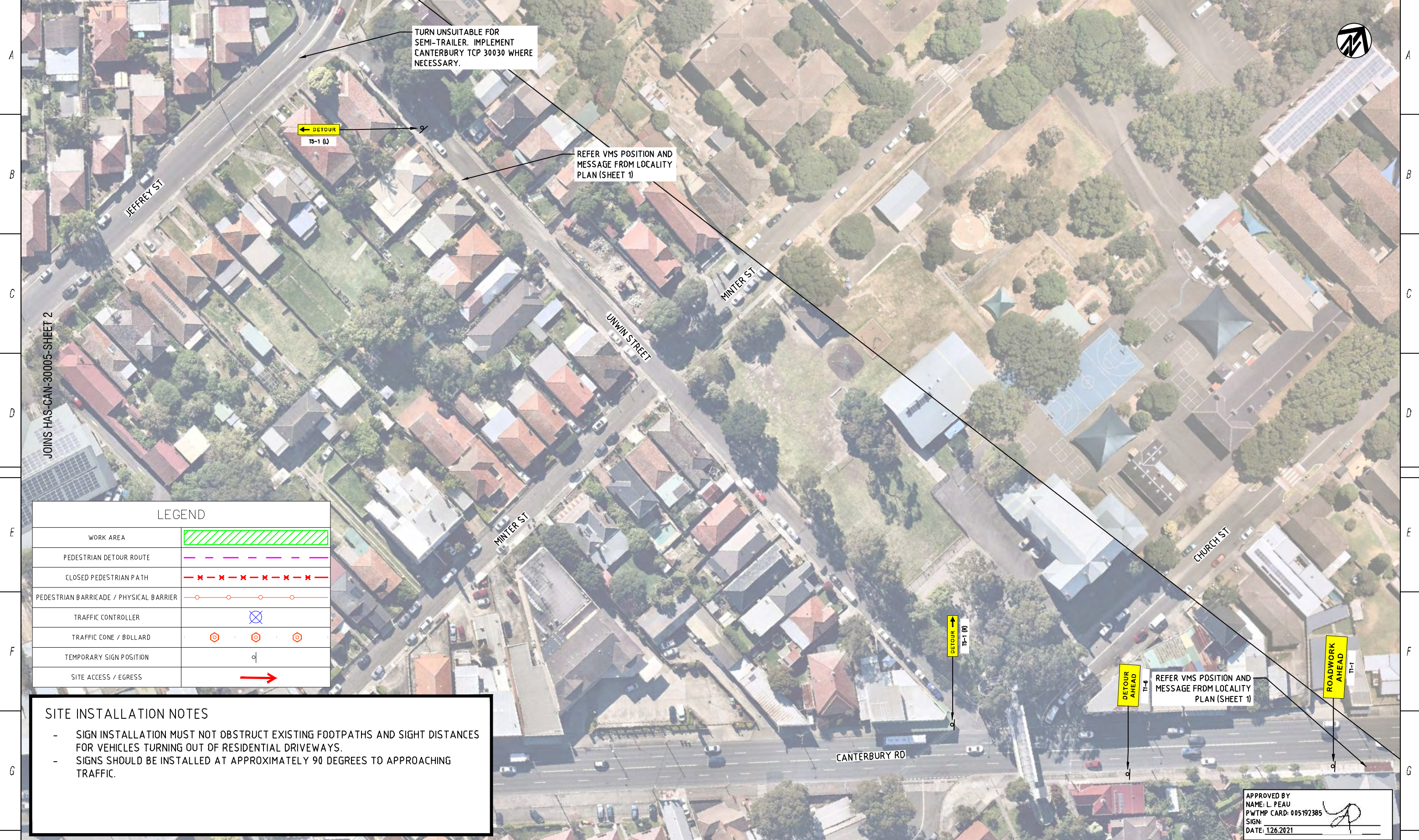
BROUGHTON STREET FULL CLOSURE AND DETOUR
FROM CANTERBURY RD TO ROBERT ST
TRAFFIC GUIDANCE SCHEME

DRAWING No: HAS-CAN-30005-P4

SHEET 2 OF 3

REVISION P1

JOINS HAS-CAN-30005-SHEET 3



LEGEND			
WORK AREA			
PEDESTRIAN DETOUR ROUTE			
CLOSED PEDESTRIAN PATH			
PEDESTRIAN BARRICADE / PHYSICAL BARRIER			
TRAFFIC CONTROLLER			
TRAFFIC CONE / BOLLARD			
TEMPORARY SIGN POSITION			
SITE ACCESS / EGRESS			

SITE INSTALLATION NOTES			
-	SIGN INSTALLATION MUST NOT OBSTRUCT EXISTING FOOTPATHS AND SIGHT DISTANCES FOR VEHICLES TURNING OUT OF RESIDENTIAL DRIVEWAYS.		
-	SIGNS SHOULD BE INSTALLED AT APPROXIMATELY 90 DEGREES TO APPROACHING TRAFFIC.		

DRAWN BY:	AG
DRW CHECK:	AG
APPROVED:	AG
IND REVIEW:	N/A

DESIGNER

CLIENT

HASLIN CONSTRUCTIONS - CANTERBURY STATION UPGRADE

BROUGHTON STREET FULL CLOSURE AND DETOUR
FROM CANTERBURY RD TO ROBERT ST
TRAFFIC GUIDANCE SCHEME

DRAWING No:	HAS-CAN-30005-P4
SHEET	3 OF 3
REVISION	P1

APPROVED BY	NAME: L. PEAU
	PWTMP CARD: 005192385
	SIGN:
	DATE: 12.6.2021



Appendix C – Community Notification

Keeping you informed

We would like to advise you of some upcoming changed traffic conditions around Canterbury Station.

As part of the upgrade of Canterbury Station to metro standards, the team will be installing new stairs and a lift shaft. In order to undertake this work safely, there will be **full road closure on Broughton Street (between Canterbury Road and Robert Street in both directions)** on the following dates:

- **8pm Friday 31 December 2021 to 5am Monday 3 January 2022 and**
- **8pm Friday 7 January to 5am Monday 10 January 2022.**

A traffic detour will be in place through Robert Street. Local access will be maintained at all times.

If you have any questions or concerns, please contact your local Place Manager below.

My contact information

Name Natalia


Email southwestmetro@transport.nsw.gov.au

Phone 1800 171 386

Date 20-Oct-2021

 **1800 171 386** Community information line open 24 hours

 **southwestmetro@transport.nsw.gov.au**

 **Sydney Metro City & Southwest,**
PO Box K659, Haymarket NSW 1240

[sydneymetro.info](https://www.sydneymetro.info)

Keeping you informed

We would like to advise you of some upcoming changed traffic conditions around Canterbury Station.

As part of the upgrade of Canterbury Station to metro standards, the team will be installing new stairs and a lift shaft. In order to undertake this work safely, there will be **full road closure on Broughton Street (between Canterbury Road and Robert Street in both directions)** on the following dates:

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If you have any questions or concerns, please contact your local Place Manager below.

My contact information

Name Natalia

Email southwestmetro@transport.nsw.gov.au

Phone 1800 171 386

Date 20-Oct-2021

 **1800 171 386** Community information line open 24 hours

 **southwestmetro@transport.nsw.gov.au**

 **Sydney Metro City & Southwest,**
PO Box K659, Haymarket NSW 1240

[sydneymetro.info](https://www.sydneymetro.info)



Translating and Interpreting Service

If you require the services of an interpreter, please contact the **Translating and Interpreting Service** on **131 450** and ask them to call **Sydney Metro** on **1800 171 386**. The interpreter will then assist you with translation.

Se avete bisogno dell'ausilio di un interprete, vi preghiamo di contattare il **Servizio di Traduzione ed Interpretariato** al numero **131 450** e chiedere di chiamare **Sydney Metro** al numero **1800 612 173**. L'interprete vi assisterà nella traduzione.

আপনার, একজন দোভাষীর (ইন্টারপ্রেটার) সেবা-সাহায্য আবশ্যিক হলে, অনুগ্রহ করে **131 450** নং এ **ট্রান্সলেটিং এন্ড ইন্টারপ্রেটিং সার্ভিস** এর সাথে যোগাযোগ করুন, এবং **1800 171 386** নং এ **সিডনী মেট্রো** কে কল করতে তাদের বলুন। তখন অনুবাদ/ভাষান্তরে, দোভাষী আপনাকে সাহায্য করবে।

如果您需要翻译服务, 请致电**131 450** 翻译和口译服务, 让他们打**1800 171 386**给悉尼地铁, 翻译员然后将帮助您进行翻译。

إذا كنتم بحاجة إلى خدمات مترجم, يرجى الاتصال بخدمة الترجمة الكتابية والشفهية على الرقم **131 450** واطلبوا منهم الاتصال بمترو سيدني على الرقم **1800 171 386**. وبعد ذلك سيقوم المترجم بمساعدتكم في الترجمة.

Εάν χρειάζεστε τις υπηρεσίες διερμηνέα, παρακαλείστε να επικοινωνήσετε με την **Υπηρεσία Μεταφραστών και Διερμηνέων** στο **131 450** και ζητήστε τους να καλέσουν το **Sydney Metro** στο **1800 171 386**. Ο διερμηνέας θα σας βοηθήσει στη μετάφραση.

통역서비스가 필요하시면, 번역 및 통역 서비스 (**Translating and Interpreting Service**) 전화 **131 450** 에 연락하시어 **Sydney Metro** 전화 **1800 171 386** 에 연결해달라고 요청하십시오. 통역관이 통역을 도와 드릴 것입니다.

Nếu quý vị cần dịch vụ thông dịch viên, xin liên lạc **Dịch vụ Thông Phiên Dịch (Translating and Interpreting)** ở số **131 450** và yêu cầu gọi **Sydney Metro** ở số **1800 171 386**. Sẽ có thông dịch viên giúp cho quý vị việc thông dịch.

यदि आपको दुभाषिए की सेवाओं की जरूरत है, तो कृपया अनुवाद एवं दुभाषिया सेवा (**Translating and Interpreting Service**) से **131 450** पर संपर्क करें और उन्हें सिडनी मेट्रो **1800 171 386** पर को फोन करने का निवेदन करें। फिर दुभाषिया अनुवाद में आपकी मदद करेगा।

*Если Вам необходима помощь переводчика, свяжитесь, пожалуйста, с переводческой службой **Translating and Interpreting Service по телефону 131 450** и попросите их соединить Вас с **Сидней Метро (Sydney Metro) по номеру 1800 171 386** Затем переводчик поможет вам с переводом*

หากท่านจำเป็นต้องใช้บริการล่าม โปรดติดต่อบริการแปลและล่าม **Translating and Interpreting Service** ที่ **131 450** และขอให้หน่วยงานดังกล่าวโทรหา **Sydney Metro** ที่ **1800 171 386** หลังจากนั้นล่ามจะช่วยท่านเกี่ยวกับการแปล

如果您需要口譯員的服務, 請致電**131 450**聯絡翻譯和口譯服務, 要求他們致電 **1800 171 386**給悉尼地鐵 (**Sydney Metro**)。然後口譯員將會協助您翻譯。



Translating and Interpreting Service

If you require the services of an interpreter, please contact the **Translating and Interpreting Service** on **131 450** and ask them to call **Sydney Metro** on **1800 171 386**. The interpreter will then assist you with translation.

Se avete bisogno dell'ausilio di un interprete, vi preghiamo di contattare il **Servizio di Traduzione ed Interpretariato** al numero **131 450** e chiedere di chiamare **Sydney Metro** al numero **1800 612 173**. L'interprete vi assisterà nella traduzione.

আপনার, একজন দোভাষীর (ইন্টারপ্রেটার) সেবা-সাহায্য আবশ্যিক হলে, অনুগ্রহ করে **131 450** নং এ **ট্রান্সলেটিং এন্ড ইন্টারপ্রেটিং সার্ভিস** এর সাথে যোগাযোগ করুন, এবং **1800 171 386** নং এ **সিডনী মেট্রো** কে কল করতে তাদের বলুন। তখন অনুবাদ/ভাষান্তরে, দোভাষী আপনাকে সাহায্য করবে।

如果您需要翻译服务, 请致电**131 450** 翻译和口译服务, 让他们打**1800 171 386**给悉尼地铁, 翻译员然后将帮助您进行翻译。

إذا كنتم بحاجة إلى خدمات مترجم, يرجى الاتصال بخدمة الترجمة الكتابية والشفهية على الرقم **131 450** واطلبوا منهم الاتصال بمترو سيدني على الرقم **1800 171 386**. وبعد ذلك سيقوم المترجم بمساعدتكم في الترجمة.

Εάν χρειάζεστε τις υπηρεσίες διερμηνέα, παρακαλείστε να επικοινωνήσετε με την **Υπηρεσία Μεταφραστών και Διερμηνέων** στο **131 450** και ζητήστε τους να καλέσουν το **Sydney Metro** στο **1800 171 386**. Ο διερμηνέας θα σας βοηθήσει στη μετάφραση.

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