



Planning Approval Consistency Assessment Form

SM-17-00000111

Metro Body of Knowledge (MBoK)

Assessment name:	SM Package 4 HSE. Temporary use of kerb side car parking spaces on Charles Street, Canterbury, for the Christmas shutdown
Prepared by:	Elena Ivanova - HSEJV
Prepared for:	Sydney Metro
Assessment number:	SWM26
Status:	Final
Version:	2.0
Planning approval:	SSI 8256
Date required:	26/12/2021
iCentral number:	SM-21-0042875
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For information – do not alter:

Applicable to:	Sydney Metro
Document Owner:	Director, Environment, Sustainability & Planning
System Owner:	Deputy Chief Executive, Operations, Customer & Place-making
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The Planning Approval Consistency Assessment Form should be completed in accordance with [SM-17-00000103 Planning Approval Consistency Assessment Procedure](#).

1. Existing Approved Project

Planning approval reference details (Application/Document No. (including modifications)):

SSI 8256 Sydney Metro City & Southwest – Sydenham to Bankstown (S2B)
Mod 1 Revised station design for Bankstown Station

Date of determination:

Infrastructure Approval date 12 December 2018

Modification 1 Approval date 22 October 2020

Type of planning approval:

Critical State Significant Infrastructure

Description of existing approved project you are assessing for consistency:

The Marrickville, Canterbury and Lakemba Station (MCL) Upgrades is one of the stages of the Sydenham to Bankstown upgrade (herein referred to as the Southwest Metro (SWM) Project) as described in the project's delivery strategy. The MCL upgrades to Sydney Metro standards correspond to work package No. 4 which are being undertaken by Haslin / Stephen Edwards Joint Venture (HSE JV).

Upgrade works at Canterbury Station involves the following:

- Refurbish and repurpose rooms of existing concourse booking office, platform building 1 and 2;
- Remove existing stair & canopy to Platform 1. Provide a new lift & stair to Platform 1 including associated canopies;
- Regrade platform as per Sydney Metro's requirement and provide drainage, platform screen doors, platform edge screens and mechanical gap fillers to Platform 1 and 2;
- Provide a new lift to platform 2 including associated canopies;
- Construction of the Sydney Metro Services Building;
- Provide new security gates to concourse entry;
- New cabling and containment for LV services and lighting;
- Clad the southern side of station concourse booking office, and refurbish the building. Provide a new opening onto Canterbury Road for existing retail;
- Remove the existing planter beds to Broughton Street;
- Remove the canopy directly over the existing planter bed facing Broughton Street;
- Remove existing brick retaining wall from station concourse forecourt entry adjacent to Canterbury Road;
- Provide accessible entries from both Canterbury Road and Broughton Street to station concourse;
- Replace the existing vertical protection (anti-throw) screens to the station concourse bridge;
- Renew lighting to the concourse, footbridge, platform buildings, platforms and ramp to Platform 2;
- Repair the existing booking office roof and associated stormwater system. Repaint, repoint and repair existing platform buildings;
- Replace existing balustrade on Platform 2 ramp and continue new fencing to platform building 2 with new. Resurface asphalt finish to Platform 2 ramp and contain asphalt edges with steel flat bar;

- Installation of new CSR cable route;
- Installation of security and segregation fencing;
- Canterbury Road bridge parapet works (city and country side); and
- Replacement of existing bus shelters on Broughton Street.

It should also be noted that the SPIR identified key changes to the construction methodology for the preferred project (compared to the exhibited project in the EIS) to reduce community impacts. This Planning Approval Consistency Assessment has been produced to assess potential impacts of temporary removal of public car parking spaces near 15 Charles Street, Canterbury associated with Canterbury Station upgrades to Sydney Metro standards, and to determine whether those impacts can be appropriately managed under the current Conditions of Approval, Revised Environmental Mitigation Measures, management plans, procedures and strategies.

Relevant background information (including EA, REF, Submissions Report, Director General's Report, MCoA):

- The Sydney Metro City & Southwest – Sydenham to Bankstown – State Significant Infrastructure Assessment (SSI 8256), dated 12th December 2018;
- The Sydney Metro City & Southwest – Sydenham to Bankstown - Environmental Impact Statement, dated 7th September 2017;
- The Sydney Metro City & Southwest – Sydenham to Bankstown – Submissions and Preferred Infrastructure Report, June 2018;
- The Sydney Metro City & Southwest – Sydenham to Bankstown – Submissions Report, September 2018;
- The Sydney Metro City & Southwest – Sydenham to Bankstown – Instrument of Approval, dated 12th December 2018; and
- The Sydney Metro City & Southwest – Sydenham to Bankstown – Modification 1 – Bankstown Station, 22nd October 2020.

All proposed works identified in this assessment would be undertaken in accordance with the mitigation measures identified in the EIS, Submissions and Preferred Infrastructure Report, the Submission Report and the conditions of approval.

2. Description of proposed development/activity/works

Describe ancillary activities, duration of work, working hours, machinery, staffing levels, impacts on utilities/authorities, wastes generated, or hazardous substances/dangerous goods used.

This Planning Approval Consistency Assessment (CA) relates to the temporary closure and use of a total of nine kerb side car parking spaces on both sides of the road near 15 Charles Street, Canterbury during the Christmas shutdown (26.12.2021 to 10.01.2022) and 2 days after possession. The kerb side car parking spaces to be used are shown in Figure 2 below.

The closure and use of these car parking spots is required to increase the construction area around the proposed Metro Services Building (MSB) area for the safe delivery of construction materials. Parking spaces along the MSB area will be used for parking of construction plant (e.g. EWP) required for unloading, while parking spaces on the other side of the road will be used to turn vehicles. Due to a low clearance of the Cooks River rail bridge, the delivery trucks have to access and egress the site compound at Charles Street through the intersection between Charles Street and Canterbury Road.

The proposed delivery will not require additional amendments to the current traffic management approved for use during possessions. Approved traffic management permits for single lane closure and full closure for a short time allowing construction plant manoeuvring between the main compound (the MSB construction area) and the compound located at 6 Charles Street. Delivery of construction materials is scheduled and only one truck will arrive at a time to minimise traffic impacts on Charles Street.

Charles Street is located within the City of Canterbury Bankstown Council Local Government Area (LGA) as shown in Figure 1 below. The footpath along the multi-storey building at 15 Charles Street will be maintained open. The pedestrian pathway along the MSB area will be closed and traffic controllers will safely re-direct pedestrians to the other side of Charles Street as detailed in the Traffic Control Plans (TCPs). Appendix A provides traffic controls planned to be in place.

Charles Street will be closed periodically for a maximum of 10 minutes to control construction traffic; however, the road will predominantly be maintained open to public traffic. The traffic controller crew will manage all construction vehicles movements between the main compound (the MSB construction area) and the compound located at 6 Charles Street, including public traffic.



Figure 1 – Location of car parking spaces on Charles Street, Canterbury

Closure of 9 kerb side car parking spaces will only be required during the Christmas shutdown works which will take place under the relevant Out of Hours Works Application prepared by HSE and approved by Sydney Metro.

Plant and equipment to be used at Charles Street and to access the rail corridor is not expected to differ from the approved construction methodology. The machinery includes, but not limited to:

- Mobile/Franna crane
- Delivery trucks (semi-trucks)
- EWP
- Structural components.

The proposed area will be used during the Christmas shutdown (26/12/2021 to 10/01/2022) associated with the project. The area would be required 2 days after the completion of the shutdown (to remove the machinery). This land is located outside, but adjacent to the existing Project Boundary as defined by the EIS/SPIR.

No change to project staffing levels is expected during the proposed works.

Upon completion of works the proposed area would be reinstated to the public.

3. Timeframe

When will the proposed change take place? For how long?

The proposed area will be used for the Christmas shutdown (26/12/2021 to 10/01/2022). It is expected that these pits will be delivered in the 2nd week of the shutdown. These dates exclude the 2 days after the Christmas shutdown period.

4. Site description

Provide a description of the site on which the proposed works are to be carried out, including, Lot and Deposited Plan details, where available. Map to be included here or as an appendix. Detail of landowner.

The kerb side car parking spaces shown in Figure 2 below, proposed for use by HSE JV, are located within the road reserve on land owned by the City of Canterbury-Bankstown Council. A standing plant permit granted by is provided in Appendix B.



Figure 2 – Car parking spaces to be closed

5. Site Environmental Characteristics

Describe the environment (i.e., vegetation, nearby waterways, land use, surrounding land use), identify likely presence of protected flora/fauna and sensitive area.

The environment at Charles Street, Canterbury can be described as typical urban street scape. The public car parking spaces are bordered by the main site compound (MSB area), gutters, a footpath and private residential multi-storey building located at 15 Charles Street. Nearby vegetation consists of planted trees on both sides of Charles Street next to the car parking area. No vegetation would be impacted as a result of the temporary possession of a number of car parking spaces. Tree protection near 15 Charles Street will be installed in the vicinity of the proposed working area to prevent damaging vegetation. Rainfall runoff from the area enters stormwater pits located within the kerb side gutter. Stormwater pits are located to the front of the car spaces to 15 Charles Street and directly north-west next to the pedestrian crossing. The stormwater pit next to the pedestrian crossing is approximately 18m away. Stormwater pits will be protected with ERSSED controls.

There is no known protected flora or fauna in the vicinity.

HSE JV will restore the car parking spaces to their pre-existing condition upon completion of the Christmas shutdown works in accordance with a dilapidation report prepared prior to construction.

6. Justification for the proposed works

Address the need for the proposed works, whether there are alternatives to the proposed works (and why these are not appropriate), and the consequences with not proceeding with the proposed work.

Currently, there are minimal available flat hardstand surfaces around the main compound (MSB area) and the rail corridor that are located within the project boundary. As such, the nine kerb side public car parking spaces near 15 Charles Street are deemed to be the most suitable location for placing construction plant, which requires a larger hard stand area to operate safely.

It will also ensure safe manoeuvring of vehicles in the MSB area and prevent Charles Street from being completely closed.

The project boundary at Canterbury includes mostly built-up areas. There would be no other feasible location to place the machinery and safely deliver construction materials to the MSB area. No rail service during the shutdown will be operating during the road closure so this would have minimal disruption to potential commuters.

7. Environmental Benefit

Identify whether there are environmental benefits associated with the proposed works. If so, provide details:

NIL

8. Control Measures

Will a project and site specific EMP be prepared? Are appropriate control measures already identified in an existing EMP?

Works will be completed under the project Construction Traffic Management Plan (CTMP), Construction Environmental Management Plan (CEMP) and sub-plans, including the Construction Noise and Vibration Management Plan (CNVMP), Construction Soil and Water Management Plan (CSWMP), and Community Consultation Strategy (CCS).

The Environmental Control Map (ECM) and Erosion and Sediment Control Plan (ESCP) will be updated accordingly to capture the temporary use of 9 kerb side car parking spaces near 15 Charles Street, Canterbury.

9. Climate Change Impacts

Is the site likely to be adversely affected by the impacts of climate change? If yes, what adaptation/mitigation measures will be incorporated into the design?

NIL

10. Impact Assessment – Construction

Attach supporting evidence in the Appendices if required. Make reference to the relevant Appendix if used.

Aspect	Nature and extent of impacts (negative and positive) during construction (if control measures implemented) of the proposed/activity, relative to the Approved Project	Proposed Control Measures in addition to project COA and REMMs	Minimal Impact Y/N	Endorsed	
				Y/N	Comments
Flora and fauna	Vegetation would not be required to be removed due to the proposal. No change from approved project as detailed in the EIS and SPIR.	Tree protection to be installed around the trees along the public car parking spaces to be occupied by HSE JV in accordance with the CEMP.	Y	Y	
Water	No change is proposed to the existing water system from approved project as detailed in the EIS and SPIR.	Implement mitigation measures as stated within the CEMP and ESCP. No additional measures required.	Y	Y	
Air quality	There will be potential localised dust impacts from vehicle and material movements, the extent to which is considered to be consistent with the impacts assessed within the EIS and SPIR	Implement mitigation measures as stated within the CEMP. No additional measures required.	Y	Y	
Noise vibration	<p>The machinery will be located approximately 20 meters from the nearest receivers. Noise monitoring will be undertaken during the Christmas shutdown to ensure compliance with the CNVIS attached to the OOHV application.</p> <p>There will be minimal noise impacts associated with unloading and turning trucks. Works will be consistent with already approved activities as detailed in the EIS and SPIR.</p> <p>The controls within the Construction Noise and Vibration Management Plan (CNVMP) address construction laydown machinery which are considered relevant to the proposal.</p>	<p>Implementation of control measures as per the CEMP, CNVMP and OOHV.</p> <p>Noise monitoring will be undertaken during the possession period.</p>	Y	Y	

Aspect	Nature and extent of impacts (negative and positive) during construction (if control measures implemented) of the proposed/activity, relative to the Approved Project	Proposed Control Measures in addition to project COA and REMMs	Minimal Impact Y/N	Endorsed	
				Y/N	Comments
Aboriginal Heritage	The temporary use of the Charles Street public car parking spaces does not go below the existing ground surface and therefore no potential to impact Aboriginal Heritage.	No additional measures required.	Y	Y	
Non-Aboriginal heritage	No heritage listed items are near the public car parking spaces along Charles Street to be used during the Christmas shutdown. Additionally, the temporary use of this land does not go below the ground surface and therefore no potential to impact archaeological heritage.	No additional measures required.	Y	Y	
Community and stakeholder	<p>The proposed temporary possession of public car parking spaces near 15 Charles Street is expected to result in inconveniences to the community associated with changed traffic conditions and reduced parking spaces.</p> <p>No private property access will be impacted by the proposed public car park closure as the machinery will be located within the car parking spots. Note that spaces to be taken are public spaces not visitor parking spaces for apartment complexes.</p>	<p>Community notification will be provided via letterbox drop and emailed prior to the shutdown period.</p> <p>Implementation of controls detailed the TCPs.</p>	Y	Y	

Aspect	Nature and extent of impacts (negative and positive) during construction (if control measures implemented) of the proposed/activity, relative to the Approved Project	Proposed Control Measures in addition to project COA and REMMs	Minimal Impact Y/N	Endorsed	
				Y/N	Comments
Traffic	<p>Charles Street is already used as the main access road to the site compounds located on both side of Charles Street, therefore, traffic levels in this area will remain consistent with the existing environment during the Canterbury Station Upgrades.</p> <p>Charles Street will be closed periodically for a maximum of 10 minutes to control construction traffic; however, the road will predominantly be maintained open to public traffic.</p> <p>The footpath along the multi-storey building at 15 Charles Street will remain open. The pedestrian pathway along the MSB area will be closed and traffic controllers will safely re-direct pedestrians to the opposite side of Charles Street.</p> <p>The temporary closure and use of a total of nine kerb side car parking spaces on both sides of the road near 15 Charles Street, Canterbury during the Christmas shutdown and 2 additional days after possession will have minor traffic impacts to the public.</p> <p>The Shutdown will be undertaken during Christmas time that is a holiday season and demand for public car parking is usually low.</p> <p>The car parking spaces near 15 Charles Street are used by public and loss of parking spaces will have minor impacts to the community.</p> <p>The proposed work would have minor additional traffic impacts to those assessed in the project EIS and SPIR.</p>	Implementation of mitigation measure as per the CTMP and the TCPs.	Y	Y	

Aspect	Nature and extent of impacts (negative and positive) during construction (if control measures implemented) of the proposed/activity, relative to the Approved Project	Proposed Control Measures in addition to project COA and REMMs	Minimal Impact Y/N	Endorsed	
				Y/N	Comments
Waste	No waste is proposed to be stored within the public car parking spaces. The waste impact in this location is consistent with the approved project. No change from the EIS and SPIR.	No additional measures required.	Y	Y	
Social	As above for Community and Stakeholder.	No additional measures required.	Y	Y	
Economic	The temporary closure and use of a total of nine kerb side car parking spaces on both sides of the road near 15 Charles Street, Canterbury during the Christmas shutdown and 2 additional days after possession will not impact local business as the nearest local cafes-shops are located approximately 200 metres away. Due to a low clearance of the Cooks River rail bridge, the delivery trucks have to access local businesses at Charles Street through the intersection between Charles Street and Canterbury Road. The proposed closure and use of car parking spaces will not impact this delivery arrangements. No change from the EIS and SPIR.	No additional measures required.	Y	Y	
Visual	Visual impacts from the spread of machinery and the like in this location will be temporary. The machinery required and visual impacts of machinery is addressed in the EIS and SPIR, visual aspects are considered to be consistent with the EIS and SPIR.	No additional measures required.	Y	Y	
Urban design	No change from the EIS and SPIR	No additional measures required.	Y	Y	
Geotechnical	No change from the EIS and SPIR	No additional measures required.	Y	Y	

Aspect	Nature and extent of impacts (negative and positive) during construction (if control measures implemented) of the proposed/activity, relative to the Approved Project	Proposed Control Measures in addition to project COA and REMMs	Minimal Impact Y/N	Endorsed	
				Y/N	Comments
Land use	The temporary change of the public carpark to allow the placement of machinery is acknowledged. The public car park spaces proposed to be used are outside, but adjacent to the Project Boundary as defined by the EIS/SPIR.	No additional measures required.	Y	Y	
Climate Change	No change from the EIS and SPIR	No additional measures required.	Y	Y	
Risk	No change from the EIS and SPIR	No additional measures required.	Y	Y	
Other	No change from the EIS and SPIR	No additional measures required.	Y	Y	
Management and mitigation measures	<p>The project Construction Environmental Management Plan (CEMP) and sub-plans, including the Construction Noise and Vibration Management Plan (CNVMP), and Community Consultation Strategy (CCS) will be updated accordingly to identify the temporary use of the nine kerb-side public car parking spaces during the Christmas shutdown as well as three days after the Christmas shutdown.</p> <p>The ECM will be updated accordingly to capture the temporary use of the public car spaces on both sides of Charles Street.</p>	No additional measures required.	Y	Y	

11. Impact Assessment – Operation

Attach supporting evidence in the Appendix if required. Make reference to the relevant Appendix if used.

Aspect	Nature and extent of impacts (negative and positive) during operation (if control measures implemented) of the proposed activity/works, relative to the Approved Project	Proposed Control Measures in addition to project COA and REMMs	Minimal Impact Y/N	Endorsed	
				Y/N	Comments
Flora and fauna	No change from the EIS and SPIR	N/A	Y	Y	
Water	No change from the EIS and SPIR	N/A	Y	Y	
Air quality	No change from the EIS and SPIR	N/A	Y	Y	
Noise vibration	No change from the EIS and SPIR	N/A	Y	Y	
Aboriginal heritage	No change from the EIS and SPIR	N/A	Y	Y	
Non-Aboriginal heritage	No change from the EIS and SPIR	N/A	Y	Y	
Community and stakeholder	No change from the EIS and SPIR	N/A	Y	Y	
Traffic	No change from the EIS and SPIR	N/A	Y	Y	
Waste	No change from the EIS and SPIR	N/A	Y	Y	
Social	No change from the EIS and SPIR	N/A	Y	Y	
Economic	No change from the EIS and SPIR	N/A	Y	Y	
Visual	No change from the EIS and SPIR	N/A	Y	Y	

Aspect	Nature and extent of impacts (negative and positive) during operation (if control measures implemented) of the proposed activity/works, relative to the Approved Project	Proposed Control Measures in addition to project COA and REMMs	Minimal Impact Y/N	Endorsed	
				Y/N	Comments
Urban design	No change from the EIS and SPIR	N/A	Y	Y	
Geotechnical	No change from the EIS and SPIR	N/A	Y	Y	
Land use	No change from the EIS and SPIR	N/A	Y	Y	
Climate Change	No change from the EIS and SPIR	N/A	Y	Y	
Risk	No change from the EIS and SPIR	N/A	Y	Y	
Other	No change from the EIS and SPIR	N/A	Y	Y	
Management and mitigation measures	No change from the EIS and SPIR	N/A	Y	Y	

12. Consistency with the Approved Project

Based on a review and understanding of the existing Approved Project and the proposed modifications, is there is a transformation of the Project?	No. The proposed works would not transform the project, they will facilitate it. The project would continue to provide a new metro rail line between Sydenham and Bankstown.
Is the project as modified consistent with the objectives and functions of the Approved Project as a whole?	Yes. The proposed works would be consistent with the objectives and functions of the approved project.
Is the project as modified consistent with the objectives and functions of elements of the Approved Project?	Yes. The changes identified in this assessment are temporary and are consistent with the objectives and functions of the Approved Project.
Are there any new environmental impacts as a result of the proposed works/modifications?	There will be short-term and minor potential community impacts with the temporary possession of the public car parking spaces on both sides of 15 Charles Street, Canterbury. No new environmental impacts are introduced as part of the proposed use of Charles Street. All potential impacts are adequately addressed through the application of the mitigation measures in the above tables, the EIS and construction environmental management plan for the project.
Is the project as modified consistent with the conditions of approval?	Yes. The proposed works would be consistent with the conditions of approval.
Are the impacts of the proposed activity/works known and understood?	Yes. The impacts of the proposed works are understood.
Are the impacts of the proposed activity/works able to be managed so as not to have an adverse impact?	Yes. The impacts of the proposed works can be managed so as to avoid an adverse impact.

13. Other Environmental Approvals

Identify all other approvals required for the project:

Canterbury-Bankstown City Council landowner consent and the Out Of Hours Works (OOHW) Approvals from Sydney Metro

Author certification

To be completed by person preparing checklist.

I certify that to the best of my knowledge this Consistency Checklist:

- Examines and takes into account the fullest extent possible all matters affecting or likely to affect the environment as a result of activities associated with the Proposed Revision; and
- Examines the consistency of the Proposed Revision with the Approved Project; is accurate in all material respects and does not omit any material information.

Name:	Elena Ivanova	Signature:	
Title:	Environmental Advisor		
Company:	HSE JV	Date:	20/10/2021

This section is for Sydney Metro only.

Application supported and submitted by

Name:	Yvette Buchli	Date:	22/12/2021
Title:	Associate Director Planning Approvals	Comments:	
Signature:			

Based on the above assessment, are the impacts and scope of the proposed activity/modification consistent with the existing Approved Project?

Yes ☒ The proposed activity/works are consistent and no further assessment is required.

No ☐ The proposed works/activity is not consistent with the Approved Project. A modification or a new activity approval/ consent is required. Advise Project Manager of appropriate alternative planning approvals pathway to be undertaken.

Endorsed by

Name:	Fil Cerone	Date:	23 December 2021
Title:	Director, City & Southwest, Sustainability Environment and Planning	Comments:	
Signature:			









Appendix A – Traffic Control Plans

SHEET 2

GENERAL NOTES

- THIS TCP HAS BEEN PREPARED IN ACCORDANCE WITH THE TCAWS MANUAL V6 2020.
- THE CONTRACTOR SHALL ENSURE ALL ROAD OCCUPANCY PERMITS AND SPEED ZONE AUTHORISATION REQUIREMENTS ARE SATISFIED PRIOR IMPLEMENTATION OF THIS TCP.
- ANY EXISTING SIGNAGE THAT CONFLICTS WITH THIS TCP MUST BE COVERED AT THE START OF OPERATION AND UNCOVERED AT THE COMPLETION.
- THE SITE MUST COMPLY WITH THE TRAFFIC CONTROL AT WORK SITES MANUAL V6 2020 EDITION AND A.S. 1742.3
- LOCATION CHECKLIST MUST BE COMPLETED FOR ALL WORKSITES
- SIGNS TO BE POSITIONED IN ACCORDANCE WITH THE TCAWS MANUAL V6 2020.
- TRAFFIC CONTROLLERS TO BE POSITIONED WHERE THEY CAN MAINTAIN A CLEAR ESCAPE PATH.
- 30km/h SPEED RESTRICTION USED WHERE 1.5m CLEARANCE CANNOT BE ACHIEVED.
- THIS TCP USE IS LIMITED TO THAT OF HASLIN CONSTRUCTIONS AND THEIR ASSOCIATED SUBCONTRACTORS, TRAFFIC CONTROL PROVIDERS AND FOR THE PROJECT REFERENCED.

LEGEND

WORK AREA	
PEDESTRIAN DETOUR ROUTE	
CLOSED PEDESTRIAN PATH	
PEDESTRIAN BARRICADE / PHYSICAL BARRIER	
TRAFFIC CONTROLLER	
TRAFFIC CONE / BOLLARD	
TEMPORARY SIGN POSITION	
SITE ACCESS / EGRESS	

DRAWN BY: AG

DRW CHECK: AG

APPROVED: AG

IND REVIEW: N/A

DESIGNER



CLIENT

HASLIN

HASLIN CONSTRUCTIONS - CANTERBURY STATION UPGRADE

CANTERBURY STATION
CHARLES STREET STOP/SLOW - DRAINAGE WORKS
LOCALITY PLAN


DRAWING No: HAS-CAN-30020-P1

SHEET 1 OF 2

REVISION P1











LEGEND	
WORK AREA	
PEDESTRIAN DETOUR ROUTE	
CLOSED PEDESTRIAN PATH	
PEDESTRIAN BARRICADE / PHYSICAL BARRIER	
TRAFFIC CONTROLLER	
TRAFFIC CONE / BOLLARD	
TEMPORARY SIGN POSITION	
SITE ACCESS / EGRESS	

						DRAWN BY: AG		<div>DESIGNER</div> <div></div>	<div>CLIENT</div> <div></div>	HASLIN CONSTRUCTIONS - CANTERBURY STATION UPGRADE		DRAWING No: HAS-CAN-30020-P1					
						DRW CHECK: AG				<div>CANTERBURY STATION CHARLES STREET STOP/SLOW - DRAINAGE WORKS TRAFFIC GUIDANCE SCHEME</div>				SHEET		2 OF 2	
						APPROVED: AG								REVISION			
H1		LP		7.03.21		OVERALL ISSUE				LP							
REV		BY		DATE		DESCRIPTION		APPRO									
COORDINATE SYSTEM:				HEIGHT DATUM:				SCALE:									

SHEET 2

GENERAL NOTES

- THIS TCP HAS BEEN PREPARED IN ACCORDANCE WITH THE TCAWS MANUAL V6 2020.
- THE CONTRACTOR SHALL ENSURE ALL ROAD OCCUPANCY PERMITS AND SPEED ZONE AUTHORISATION REQUIREMENTS ARE SATISFIED PRIOR IMPLEMENTATION OF THIS TCP.
- ANY EXISTING SIGNAGE THAT CONFLICTS WITH THIS TCP MUST BE COVERED AT THE START OF OPERATION AND UNCOVERED AT THE COMPLETION.
- THE SITE MUST COMPLY WITH THE TRAFFIC CONTROL AT WORK SITES MANUAL V6 2020 EDITION AND A.S. 1742.3
- LOCATION CHECKLIST MUST BE COMPLETED FOR ALL WORKSITES
- SIGNS TO BE POSITIONED IN ACCORDANCE WITH THE TCAWS MANUAL V6 2020.
- TRAFFIC CONTROLLERS TO BE POSITIONED WHERE THEY CAN MAINTAIN A CLEAR ESCAPE PATH.
- 30km/h SPEED RESTRICTION USED WHERE 1.5m CLEARANCE CANNOT BE ACHIEVED.
- THIS TCP USE IS LIMITED TO THAT OF HASLIN CONSTRUCTIONS AND THEIR ASSOCIATED SUBCONTRACTORS, TRAFFIC CONTROL PROVIDERS AND FOR THE PROJECT REFERENCED.

LEGEND	
WORK AREA	
PEDESTRIAN DETOUR ROUTE	
CLOSED PEDESTRIAN PATH	
PEDESTRIAN BARRICADE / PHYSICAL BARRIER	
TRAFFIC CONTROLLER	
TRAFFIC CONE / BOLLARD	
TEMPORARY SIGN POSITION	
SITE ACCESS / EGRESS	

DRAWN BY:	AG
DRW CHECK:	AG
APPROVED:	AG
IND REVIEW:	N/A



HASLIN

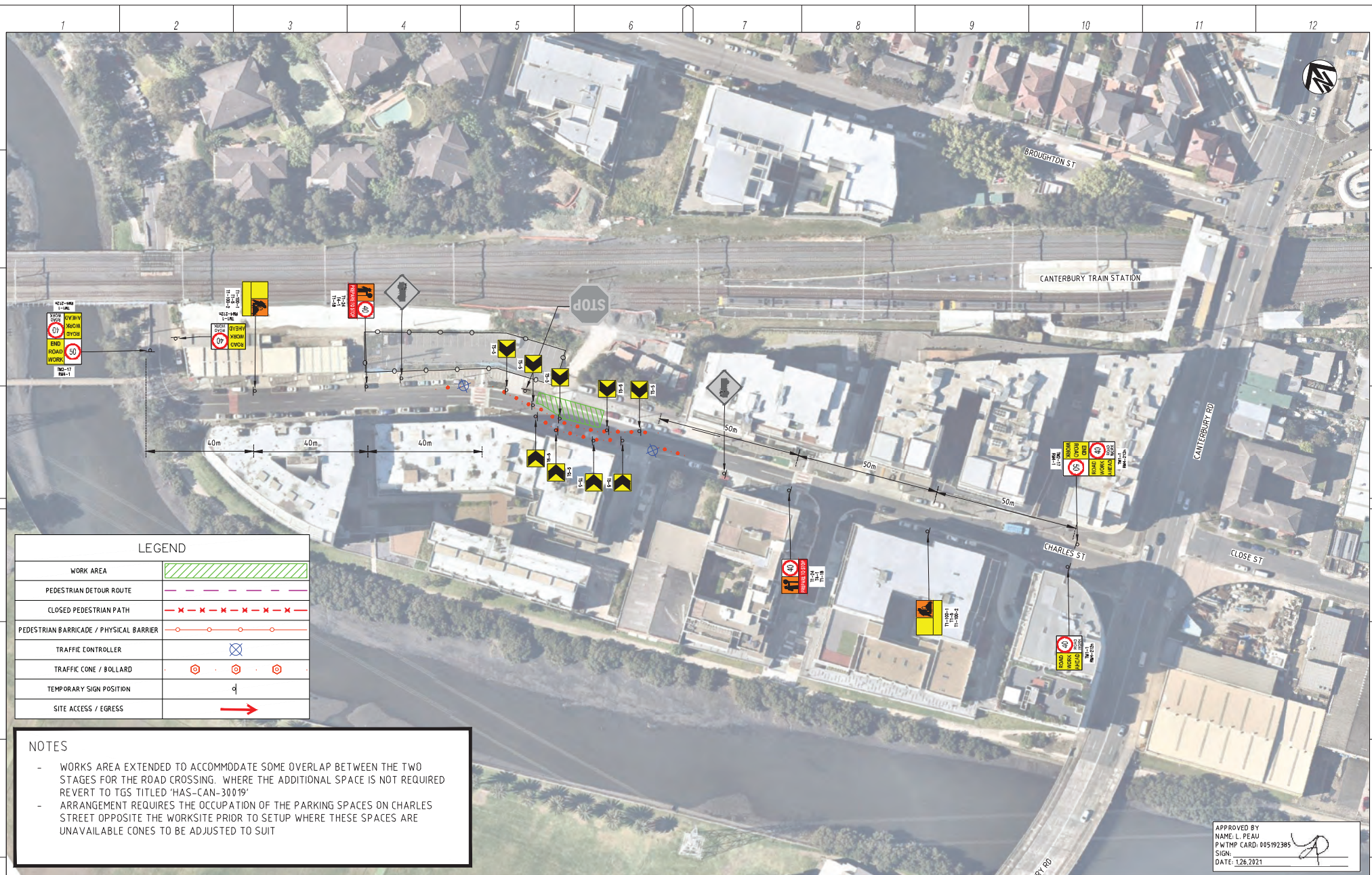
HASLIN CONSTRUCTIONS - CANTERBURY STATION UPGRADE



CANTERBURY STATION
CHARLES STREET STOP/SLOW - ROAD CROSSING PART 1
LOCALITY PLAN

DRAWING No: HAS-CAN-30021-P1

SHEET 1 OF 2

REVISION P1



			DRAWN BY: AG		<div>DESIGNER</div> <div></div>	<div>CLIENT</div> <div></div>	HASLIN CONSTRUCTIONS - CANTERBURY STATION UPGRADE		DRAWING No: HAS-CAN-30021-P1	
			DRW CHECK: AG				CANTERBURY STATION CHARLES STREET STOP/SLOW - ROAD CROSSING PART 1 TRAFFIC GUIDANCE SCHEME		SHEET 2 OF 2	
			APPROVED: AG						REVISION P1	
			IND REVIEW: N/A							
P1	LP	7.03.21	ORIGINAL ISSUE		LP					
REV	BY	DATE	DESCRIPTION		APPRO.					
		COORDINATE SYSTEM:		HEIGHT DATUM:	SCALE:					

Appendix B – Standing Plant Permit



22 November 2021

Haslin Construction
Joshua Ah Tong
2/2-4 Merton Street
SUTHERLAND NSW 2232

Our Ref: WP-CON-2327/2021
Officer: Julian Tan

Dear Sir,

Standing Plant Permit	
Work Permit Number:	WP-CON-2327/2021
Site Address:	Charles Street, CANTERBURY NSW 2193
Activity:	Concrete Pump activity relating to Canterbury Station Upgrade Works (Sydney Metro Project)
Specific Location:	Between Canterbury Road & Broughton Street CANTERBURY NSW 2193
Date & Hours:	From 01 December 2021 – 01 March 2022 Between 00:00 and 00:00 (24 Hours)

Your Work Permits for the above location have been approved. The following conditions have been imposed to ensure compliance with the Roads Act 1993 and/or the Local Government Act 1993 for the safety of persons using the public footpath and roadway adjacent to the site:

CONDITIONS OF APPROVAL

- 1) If required, a Police Permit must be obtained from the Local Police Station. Please present this letter to the Police when applying for a permit.
- 2) Carry out all work in accordance with SafeWork NSW authority's requirements.
- 3) A thoroughfare for emergency vehicles must be provided at all times.
- 4) All affected residents, businesses, emergency authorities and service providers must be notified of the work and road occupancy prior to commencement. Any concerns or requirements raised by business proprietors, residents or other occupants must be resolved or accommodated.
- 5) A current Public Liability Insurance Policy of at least \$20 million, and indicating Council as the interested party, shall be maintained during the time Council's Road Reserve is occupied.
- 6) All work is to be carried out in accordance with Australian Standards AS1742.3 – Traffic Control Devices for works on roads and in accordance with the latest version of Traffic Control and Worksites Manual (TCAWs).
- 7) The Traffic Control Plan and/or Pedestrian and Traffic Management Details as specified in your application must be implemented.
- 8) A minimum 3 metre traffic lane width must be maintained at all times.



- 9) The alternative pedestrian access route must be safe, clearly visible, appropriately delineated/ barricaded and signed at all times.
- 10) Adequate lighting and reflective devices shall be provided around the work area to ensure clear visibility to traffic and pedestrians
- 11) All control of traffic must be performed by official TfNSW authorised Traffic Controller(s).
- 12) A copy of this Permit and approved Traffic Control Plan must be kept on the work site for inspection by relevant Council Officers or the Police.
- 13) The applicant shall comply with any reasonable directive from Council's Compliance Officers or the Police.
- 14) The areas to be used for the activities must be maintained in a clean and tidy condition to the satisfaction of Council's Compliance Officer.

NOTE

(Note: This advice does not form a condition of approval)

Damage to public infrastructure (utility services or others) may result in interruption to the provision of essential services at significant costs. If you are aware of any work or proposed work which may affect or impact on public infrastructure in any way, you are required to contact the relevant authority.

For further information, please contact **Work Permit Team** of Council's Development Engineering Services via email workpermits@cbc.city.nsw.gov.au.

Yours faithfully,

A handwritten signature in black ink, appearing to read "Julian Tan", written over a horizontal line.

Julian Tan
WORK PERMIT OFFICER
DEVELOPMENT ENGINEERING SERVICES